

AMENDMENT NO. 21
TO THE OFFICIAL PLAN
FOR THE TOWN OF HALTON HILLS

Halton Peel Boundary Area Transportation Study/
GTA West Corridor Protection

THE CORPORATION OF THE TOWN OF HALTON HILLS

BY-LAW NO. 2014-

A By-law to adopt Amendment No. 21 to the Official Plan of the Town of Halton Hills – Halton Peel Boundary Area Transportation Study/ GTA West Corridor Protection

The Council of the Corporation of the Town of Halton Hills, in accordance with the provisions of the Planning Act, 1990, R.S.O., c.P. 13, as amended, hereby enacts as follows:

1. That Amendment No. 21 to the Official Plan of the Town of Halton Hills, being the attached text and schedules, is hereby adopted.
2. That the Town Clerk is hereby authorized and directed to make application to the Region of Halton for the approval of Amendment No. 21 to the Official Plan of the Town of Halton Hills.
3. That this By-law shall not come into force or take effect until Amendment No. 21 to the Official Plan for the Town of Halton Hills is approved by the Region of Halton.

BY-LAW read and passed by the Council of the Town of Halton Hills this ___ day of _____, 2014.

MAYOR – R. Bonnette

CLERK – S. Jones

AMENDMENT NO. 21 TO THE OFFICIAL PLAN
OF THE TOWN OF HALTON HILLS

The attached text and schedules constitute Amendment No. 21 to the Official Plan of the Town of Halton Hills, which was adopted by the Council of the Town of Halton Hills by By-law 2014-XXXX in accordance with the provisions of the Planning Act, 1990, R.S.O., c.P. 13, as amended:

THE CORPORATION OF THE TOWN OF HALTON HILLS.

MAYOR – R. Bonnette

CLERK – S. Jones

AMENDMENT NO. 21

TO THE OFFICIAL PLAN FOR THE TOWN OF HALTON HILLS

PART A – THE PREAMBLE does not constitute part of the Amendment.

PART B - THE AMENDMENT, consisting of the following text and schedules, constitutes Amendment No. 21 to the Official Plan for the Town of Halton Hills.

PART C – THE APPENDICES, does not constitute part of the Amendment, but is included for information purposes only.

Part A – The Preamble

1. Purpose of the Amendment

The purpose of this Amendment is to implement a corridor protection area in Halton Hills and associated land use policies that protect the area from development pending the completion of Phase 2 of the Greater Toronto Area (GTA) West Environmental Assessment and subsequent environmental assessments identified in the Halton Peel Boundary Area Transportation Study (HPBATS).

The Amendment contains the following revisions to the Official Plan necessary to implement corridor protection consistent with Provincial expectations for the GTA West Environmental Assessment and the approved recommendations of HPBATS:

- Establishment of corridor protection policies related to an HPBATS/GTA West Corridor Protection Area in the Transportation section of the Official Plan;
- Addition of an HPBATS/GTA West Corridor Protection Area as an overlay of a portion of the *Agricultural Area*, shown on Schedule A1 – Land Use Plan;
- Addition of an HPBATS/GTA West Corridor Protection Area as an overlay of the Southeast Georgetown *Future Employment Area*, shown on Schedule A3 – Georgetown Land Use;
- Addition of an HPBATS/GTA West Corridor Protection Area as an overlay of a portion of the *Premier Gateway Employment Area*, shown on Schedule A8 – Premier Gateway Employment Area;
- Addition of an HPBATS/GTA West Corridor Protection Area as an overlay of the Town's major road network, shown on Schedule B1 – Functional Plan of Major Transportation Facilities.

2. Location

The lands affected by this Amendment generally include:

- lands within the GTA West Preliminary Route Planning Study Area, in the *Agricultural Area*, east of the rear lot lines of lots fronting on Eighth Line;
- lands within the GTA West Preliminary Route Planning Study Area, in the *Premier Gateway Employment Area*, east of the Toronto Premium Outlets; and,
- lands in the vicinity of Tenth Line and Ten Side Road, including the Southeast Georgetown *Future Residential/Mixed Use Area* lands, identified in the Recommended Road Network of HPBATS.

3. Basis of the Amendment

The Halton Peel Boundary Area Transportation Study (HPBATS) was initiated in March 2007 to examine the transportation requirements along the northern Halton-Peel boundary, encompassing the eastern part of the Town of Halton Hills. The execution of a memorandum of understanding signed by Halton Region, Town of Halton Hills, City of Brampton, Town of Caledon and the Region of Peel, established a framework for the implementation of the recommended transportation improvements, a new North-South Halton-Peel Freeway Corridor. An East-West Connection from Bovaird Drive in Brampton west of the Halton-Peel Freeway to Georgetown, by-passing the Hamlet of Norval was also a critical component of the study from the viewpoint of the Town of Halton Hills and Region of Halton.

In March 2008, the Ontario Ministry of Transportation (MTO) initiated the first phase of an Environmental Assessment (EA) Study for the Greater Toronto Area West Corridor extending north of Highway 401 from the City of Guelph to the City of Vaughan. To address concerns over the impact on rural communities and agricultural resources in Halton Hills, the MTO completed additional work in Phase 1 of the EA that focused on widening Highway 401 as an alternative to a new corridor through Halton Hills.

A final Transportation Development Strategy report was released in November 2012 identifying a North-South corridor similar to the corridor recommendation in HPBATS. The GTA West corridor study recognizes the HPBATS North-South corridor as a Provincial facility generally corresponding to the geographic area identified for the HPBATS corridor. MTO has started Phase 2 of the EA process for the corridor that will focus on identifying the route and developing the preliminary design for the highway, which will result in the narrowing of the lands affected by corridor protection.

Employment Land Re-phasing

As a consequence of the implementation of corridor protection on all lands within the GTA West Preliminary Route Planning Study Area, all lands east of the Toronto Premium Outlets within the Premier Gateway (401/407 Corridor) Employment Area phased for development to the 2021 planning horizon will now be unavailable for development until the GTA West EA process determines they are not required for the transportation corridor. It is anticipated that a significant amount of this area will be permanently precluded from development in order to accommodate a large highway interchange.

Therefore, a critical complementary initiative to this HPBATS/GTA West corridor protection amendment are proposed modifications to Official Plan Amendment No. 10 of the Town of Halton Hills, which will achieve the following:

- Re-phase all employment lands to the 2021 planning horizon affected by corridor protection (i.e. inside the GTA West Preliminary Route Planning Study Area to between 2021 and 2031;
- Re-phase all employment lands to the 2031 planning horizon outside of the GTA West Preliminary Route Planning Study Area to the 2021 planning horizon, thereby permitting the initiation of a secondary planning exercise in 2014 for this area to facilitate timely development of these lands as a replacement for those subject to corridor protection;
- Identify all 2031 employment lands subject to corridor protection as a second phase of employment lands to the 2031 planning horizon, for which a separate secondary planning exercise and infrastructure planning will be required.

It is important to note that since the re-phasing of employment lands described above does not entirely replace the employment lands unavailable for development pending the completion of Phase 2 of the GTA West EA, an important element of the secondary planning exercise for the re-phased 2021 employment lands will be to evaluate alternative locations for the accommodation of the 75 ha shortfall of employment land, which would be added to the Premier Gateway Employment Area through amendment to the Region and Town Official Plans.

Part B – The Amendment

All of this part of the document entitled PART B – THE AMENDMENT, consisting of the following text and schedules, constitutes Amendment No. 21 to the Official Plan for the Town of Halton Hills.

Details of the Amendment

1. That Section F6 Transportation is amended by deleting Section F6.5 Inter-Municipal Transportation Studies and replacing it with the following:

“F6.5 HPBATS/GTA West Corridor Protection Area

F6.5.1 OBJECTIVE

It is the objective of this overlay to protect the lands subject to this overlay from *development* that could preclude or negatively affect the future use of the corridor for the purposes for which it was identified and is being planned, until the completion of the GTA West Environmental Assessment (EA) and a subsequent environmental assessment for the East-West connection identified in the Halton Peel Boundary Area Transportation Study (HPBATS).

F6.5.2 LOCATION

The *HPBATS/GTA West Corridor Protection Area* overlay as shown on Schedules A1, A3, A8 and B1 to this Plan applies to an area which corresponds with the Preliminary Route Planning Study Area as identified by the Ministry of Transportation (MTO) through Phase 1 of the GTA West EA process, as well as the Southeast Georgetown lands, which the HPBATS 2031 Recommended Road Network identifies as required for the East-West connection.

F6.5.3 LAND USE POLICIES

It is a policy of this Plan to, not complete the processing of specific *development* approvals, and prohibit the development of urban employment lands, within the *HPBATS/GTA West Corridor Protection Area* overlay until a determination is made that the lands, or any portion of the lands, are not required to accommodate the

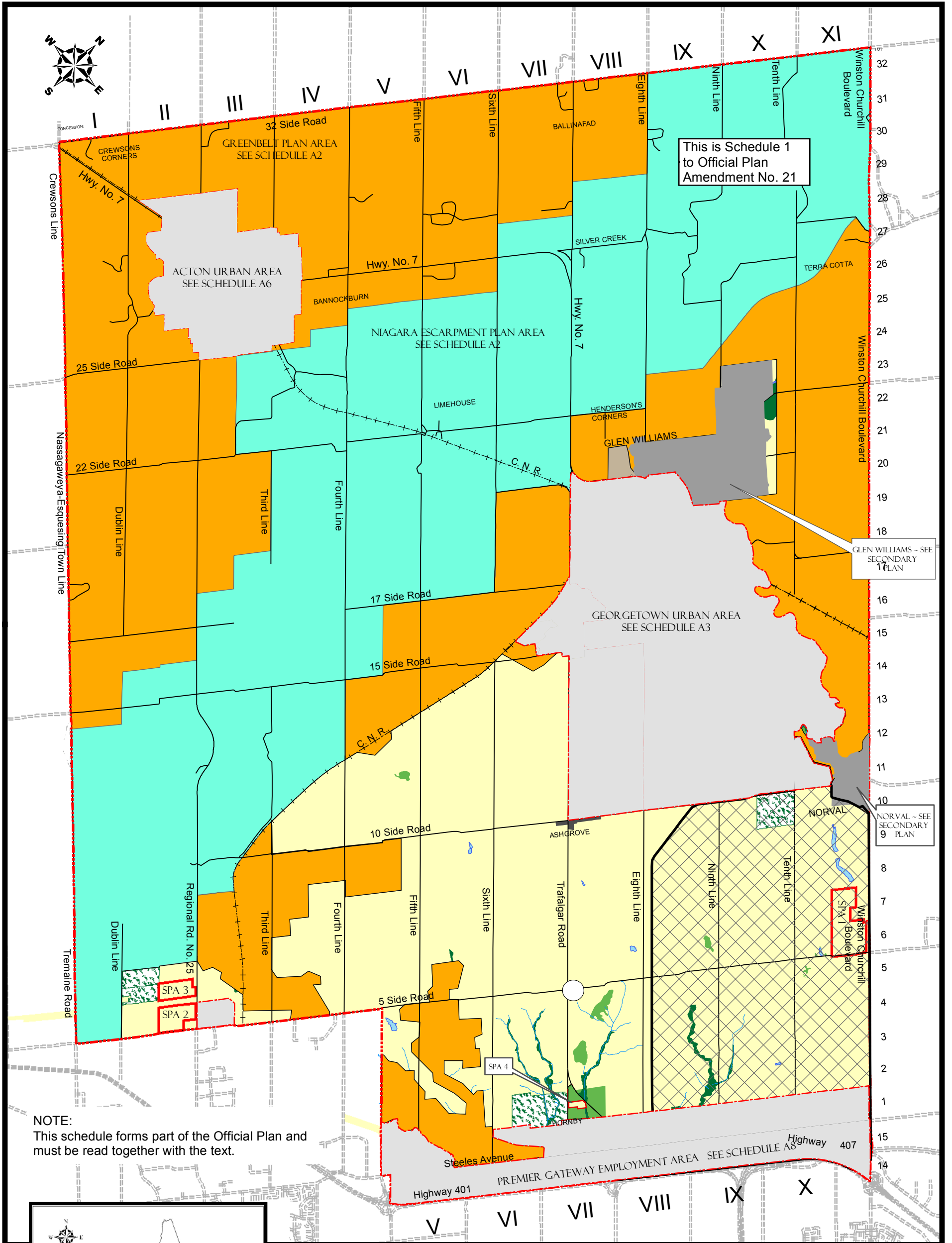
future GTA West transportation corridor or East-West connection.”

2. That Schedule A1 – Land Use Plan, as shown in Official Plan Amendment No. 10 as modified by the Region of Halton, is amended by showing an “HPBATS/GTA West Corridor Protection Area” as an overlay of lands designated *Agricultural Area, Greenlands A, Greenlands B and Private Open Space*, as shown on Schedule “1” attached to and forming part of this amendment.
3. That Schedule A3 – Georgetown Land Use, as shown in Official Plan Amendment No. 10 as modified by the Region of Halton, is amended by showing an “HPBATS/GTA West Corridor Protection Area” as an overlay of lands designated *Future Residential/Mixed Use Area*, as shown on Schedule “2” attached to and forming part of this amendment.
4. That Schedule A8 – Premier Gateway Employment Area, as shown in Official Plan Amendment No. 10 as modified by the Region of Halton, is amended by showing an “HPBATS/GTA West Corridor Protection Area” as an overlay of lands designated *Premier Gateway Employment Area Phase 2A and Premier Gateway Employment Area Phase 2B*, as shown on Schedule “3” attached to and forming part of this amendment.
5. That Schedule B1 – Functional Plan of Major Transportation Facilities as shown in Official Plan Amendment No. 10 as modified by the Region of Halton, is amended by showing an “HPBATS/GTA West Corridor Protection Area” as shown on Schedule “4” attached to and forming part of this amendment.

PART C
THE APPENDICES

To be determined

DRAFT



NOTE:
This schedule forms part of the Official Plan and must be read together with the text.



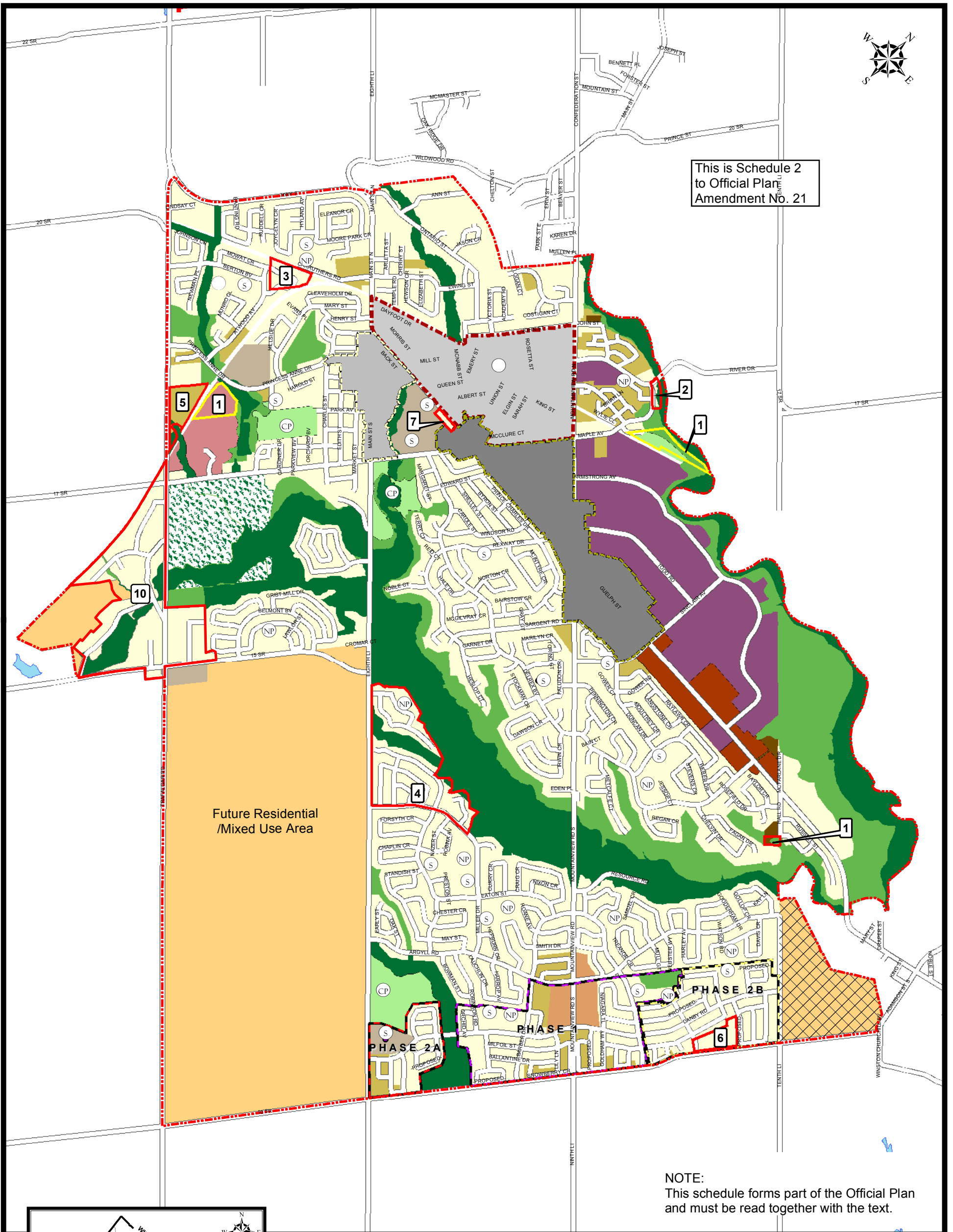
- LEGEND**
- GREENLANDS A
 - GREENLANDS B
 - PRIVATE OPEN SPACE
 - AGRICULTURAL AREA
 - MAJOR INSTITUTIONAL AREA
 - URBAN AREA
 - HAMLET AREA
 - RURAL CLUSTER AREA
 - GENERAL EMPLOYMENT AREA

- GREENBELT PLAN AREA - SEE SCHEDULE A2
- PROTECTED COUNTRYSIDE AREA
- NIAGARA ESCARPMENT PLAN AREA
- S SCHOOL
- BOUNDARIES**
- TOWN OF HALTON HILLS BOUNDARY
- SPECIAL POLICY AREA
- HPBATS/GTA WEST CORRIDOR PROTECTION AREA





This is Schedule 2
to Official Plan
Amendment No. 21



Future Residential
/Mixed Use Area

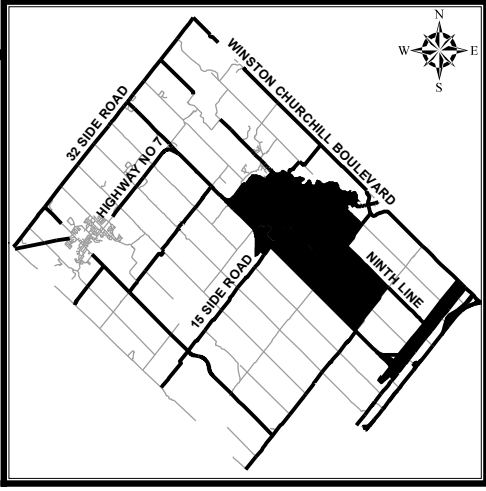
PHASE 2A

PHASE 1

PHASE 2B

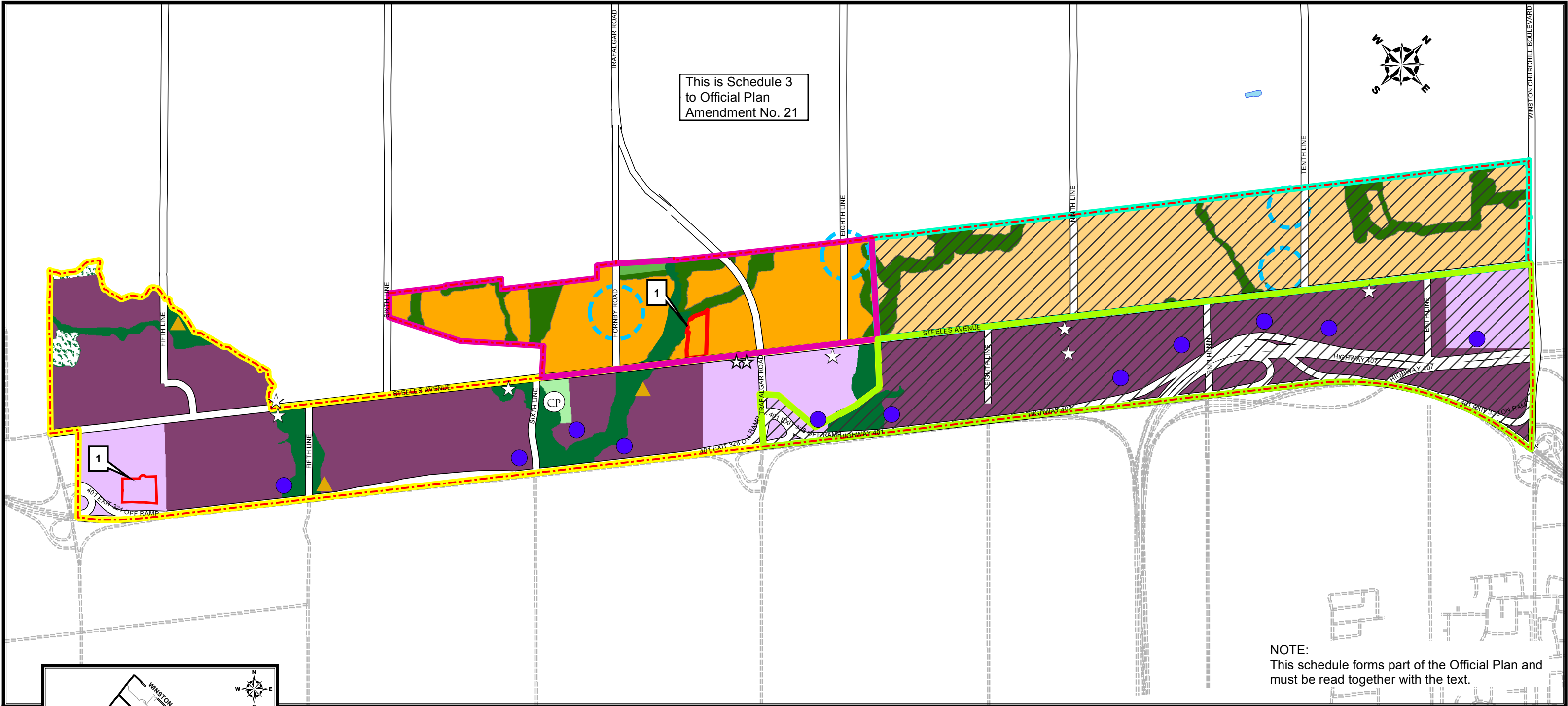
NOTE:
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and must be read together with the text.

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|---|--|--|
| <p>LIVING AREA</p> <ul style="list-style-type: none"> LOW DENSITY RESIDENTIAL AREA MEDIUM DENSITY RESIDENTIAL AREA HIGH DENSITY RESIDENTIAL AREA <p>COMMUNITY AREA</p> <ul style="list-style-type: none"> DOWNTOWN AREA - SEE SCHEDULE A4 COMMUNITY NODE - SEE SCHEDULE A5 CORRIDOR COMMERCIAL AREA SECONDARY NODE MAJOR INSTITUTIONAL AREA GENERAL EMPLOYMENT AREA CIVIC CENTRE AREA | <p>ENVIRONMENT & OPEN SPACE</p> <ul style="list-style-type: none"> GREENLANDS A GREENLANDS B MAJOR PARKS AND OPEN SPACE GREENLANDS PRIVATE OPEN SPACE COMMUNITY PARK NEIGHBOURHOOD PARK SCHOOL RESIDENTIAL SPECIAL POLICY AREA OTHER SPECIAL POLICY AREA FUTURE RESIDENTIAL/MIXED USE AREA | <ul style="list-style-type: none"> GO STATION AREA BOUNDARY - SEE SCHEDULE H3.2 DOWNTOWN AREA BOUNDARY COMMUNITY NODE BOUNDARY URBAN BOUNDARY PHASE 1 PHASE 2A PHASE 2B |
|---|--|--|
- Metres
0 50 100 200 300 400 500
- HPBATS/GTA WEST
CORRIDOR PROTECTION AREA





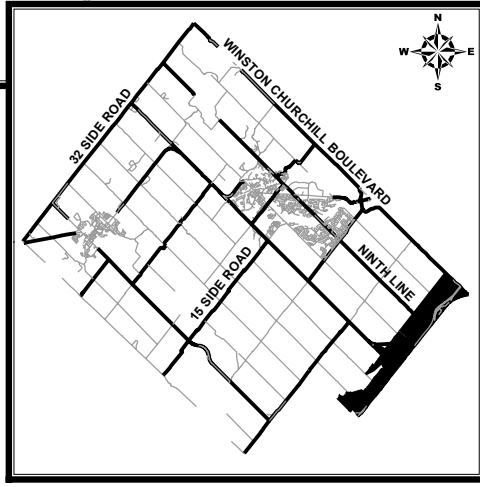
This is Schedule 3
to Official Plan
Amendment No. 21



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NOTE:
This schedule forms part of the Official Plan and
must be read together with the text.



LEGEND

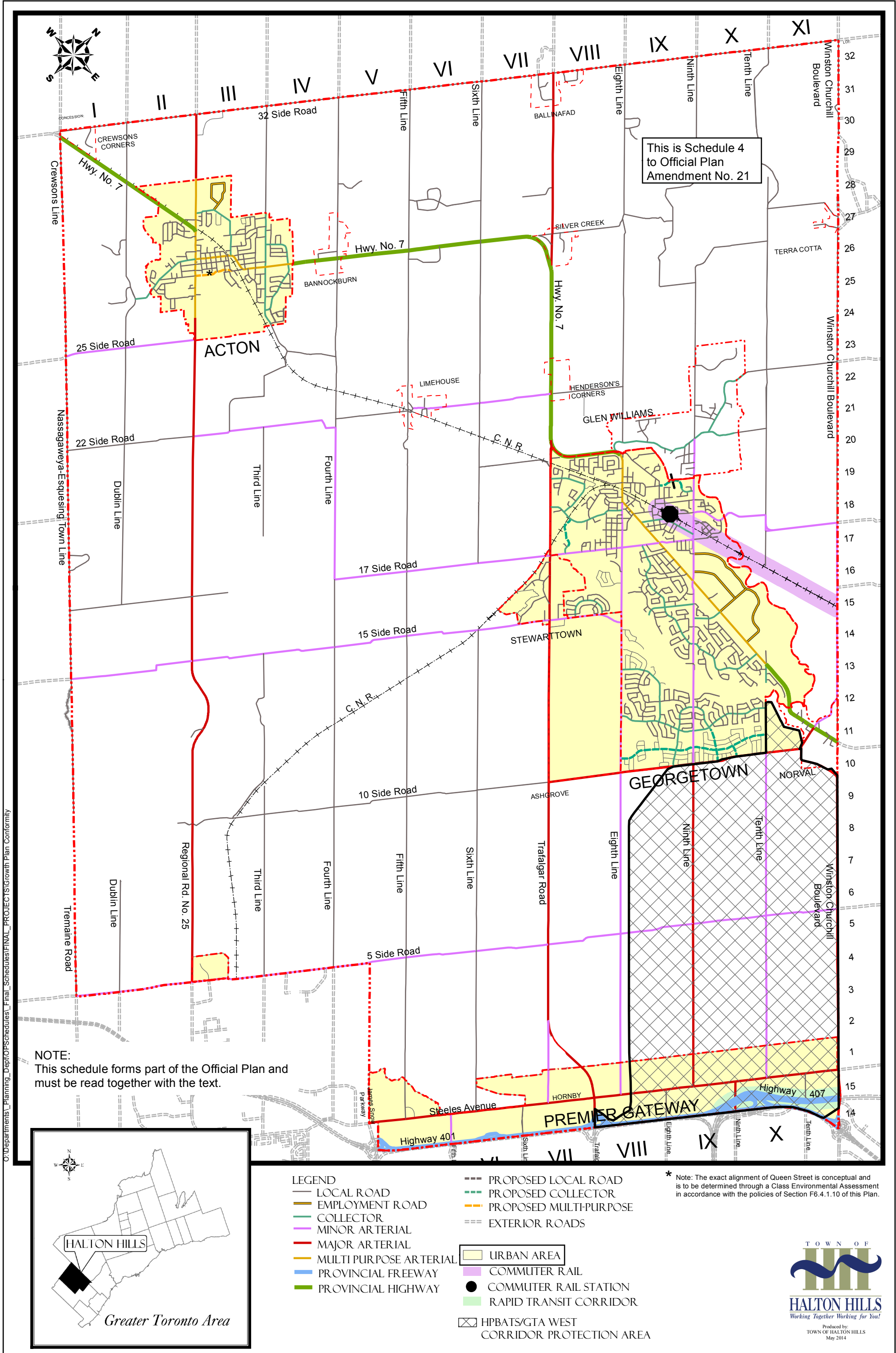
- EMPLOYMENT AREA**
- GATEWAY
 - PRESTIGE INDUSTRIAL
 - PHASE 1B EMPLOYMENT AREA
 - PHASE 2B EMPLOYMENT AREA
- ENVIRONMENT & OPEN SPACE**
- MAJOR PARKS & OPEN SPACE
 - PRIVATE OPEN SPACE
 - GREENLANDS A
 - GREENLANDS B

- CP COMMUNITY PARK
- STORMWATER MANAGEMENT POND
- POTENTIALLY UNSTABLE SLOPE
- ☆ BUILDING WITH HISTORIC SIGNIFICANCE
- URBAN BOUNDARY
- SPECIAL POLICY AREA
- EXISTING RURAL RESIDENTIAL CONCENTRATIONS

- HPBATS/GTA WEST
CORRIDOR PROTECTION AREA
- EMPLOYMENT AREA PHASING**
- PHASE 1A
 - PHASE 1B
 - PHASE 2A
 - PHASE 2B



FUNCTIONAL PLAN OF MAJOR TRANSPORTATION FACILITIES



This is Schedule 4 to Official Plan Amendment No. 21

NOTE:
This schedule forms part of the Official Plan and must be read together with the text.

* Note: The exact alignment of Queen Street is conceptual and is to be determined through a Class Environmental Assessment in accordance with the policies of Section F6.4.1.10 of this Plan.

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|---|---|
| <p>LEGEND</p> <ul style="list-style-type: none"> — LOCAL ROAD — EMPLOYMENT ROAD — COLLECTOR — MINOR ARTERIAL — MAJOR ARTERIAL — MULTI PURPOSE ARTERIAL — PROVINCIAL FREEWAY — PROVINCIAL HIGHWAY | <ul style="list-style-type: none"> --- PROPOSED LOCAL ROAD --- PROPOSED COLLECTOR --- PROPOSED MULTI-PURPOSE --- EXTERIOR ROADS |
| <ul style="list-style-type: none"> ■ URBAN AREA ● COMMUTER RAIL STATION ■ RAPID TRANSIT CORRIDOR | <ul style="list-style-type: none"> ■ HPBATS/GTA WEST CORRIDOR PROTECTION AREA |



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